

Section: Public Works	Policy Number: 4.14
Policy: Sidewalks	Effective Date: 11/16/2005
Date Last Revised:	Current Revision Date: 10/02/2018

Minto Sidewalk Policy

1.0 Purpose

Establish policy and practice regarding maintaining and improving Minto's sidewalk infrastructure.

2.0 Context

Sidewalks are an important part of a municipality's transportation infrastructure. Pedestrian movement as a means of active transportation is integral to building community.

The Strategic Plan 2018 update has numerous references to active community, maintaining infrastructure and includes the following two specific actions

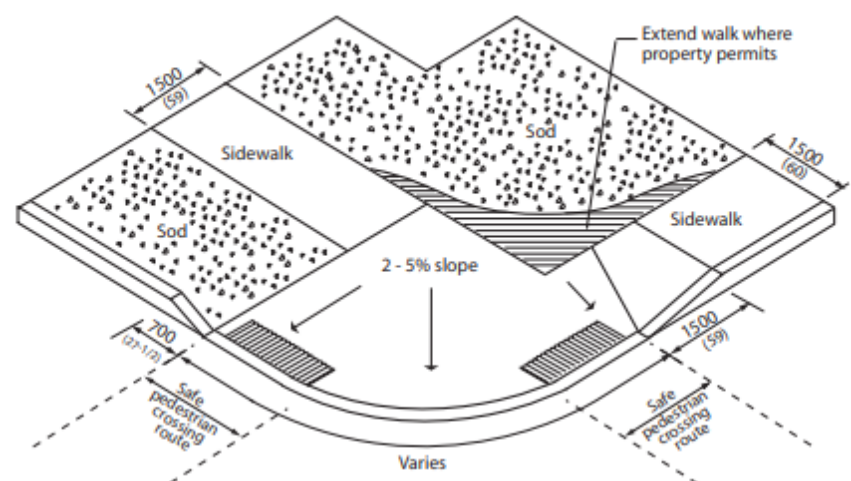
- 6.8 Implement planning standards for healthy community development including such tools as physical activity plans, walkable communities, remediation of contaminated sites, active transportation and similar.
- 7.1 Create a supportive pedestrian and cycling environment by pursuing active transportation and walkable community initiatives in accordance with County and local plans and in cooperation with local groups and organizations.

The Town of Minto was received a WALK Friendly Community designations of bronze at ACT Canada's Sustainable Mobility and Healthy Communities Summit in 2014. The WALK Friendly Communities designations encourage "practical steps to make walking safe, convenient and enjoyable. Walk friendly environments are linked to higher rates of everyday walking, helping to create vibrant neighbourhoods, and increased retail sales". Walking helps people improve their health and fitness.

2.1 Accessibility

Sidewalks are important to community accessibility for persons of all abilities. The Town adopted accessibility standards in the Wellington County Design Manual. Elements of the County standard for accessible sidewalks are included in the appendices of this policy. Some basic design requirements such as minimum 1.5 met width, 2-5% curb

Curb Ramp at Sidewalks with Narrow Boulevards



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drops to roadway and tactile strips at crossing points are shown.

2.2 Minimum Maintenance Standards

May 3rd, 2018 the former Minister of Transportation approved Ontario Regulation 366/18 enhancing Minimum Maintenance Standards for Municipal Highways. Section 16.3 requires snow accumulation on sidewalks not exceed 8cm within 48 hours of a snow event. Standard winter maintenance width shall be no less than one metre. This means that maintenance must occur where snow on sidewalks exceeds 8cm or where there is a “substantial probability” of ice formation on a sidewalk within 48 hours of an event “if practicable”.

Section 16.7 states that there is a substantial probability of snow or ice accumulation on sidewalks patrols are to be conducted. The municipality can select sidewalks it deems "as representative" and the intervals for patrols “deemed necessary by the municipality.”

Section 16.2(1) requires areas beside sidewalks are to be inspected for encroachments each year with no less than 16 months between. Encroachments are obstructions or features the Town does not install and constitute a significant hazard to pedestrians and must be removed within 28 days.

The Minimum Maintenance Standard does not provide for a municipality maintaining some sidewalks in the winter while not maintaining others. Currently the Town does not maintain a number of sidewalk sections due to their substandard width of 1.2 metres and their location. This policy will ensure over time the Town provides winter maintenance on all sidewalks that are of an appropriate standard.

2.3 Current Maintenance Standards

The Town has about 43 km of sidewalks of a variety of widths and conditions. Some older sidewalks are 1.2 metres wide do not meet accessibility standards. Cracks or broken sidewalk sections 1.2 metres wide are maintained and repaired where possible. Every spring boulevard restoration is required as present-day snow removal equipment can damage grass along sidewalk edges. Since 2012 the Town has replaced substandard sidewalks with 1.5 metre wide installations with accessible features.

For winter maintenance the Town has plowed sidewalks on main routes leading to major institutions regardless of the width or standard. The Town’s 2015 Snow Removal Policy includes designated sidewalk routes for winter control. During weekdays Town staff currently clears to less than 2.5 cm depth with sand and salt within 12 hours of an event particularly on direct routes to schools and on main roads. In almost all cases sidewalks the Town maintains in winter are completed to less than 2.5 cm before noon when there is an overnight event. The Town does not maintain sidewalks in the winter that are off main

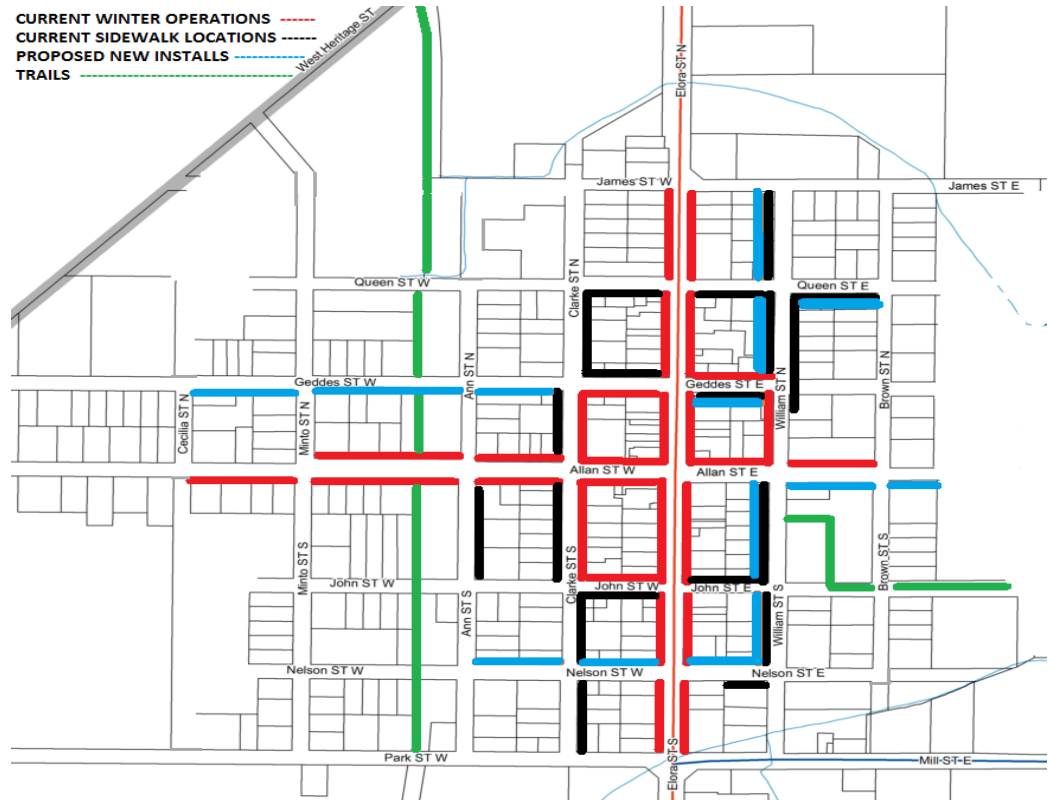
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roads, infrequently used or not direct routes to school. As a result of the new minimum maintenance standards the Town will need to move toward winter maintenance of all sidewalks to a lower standard.

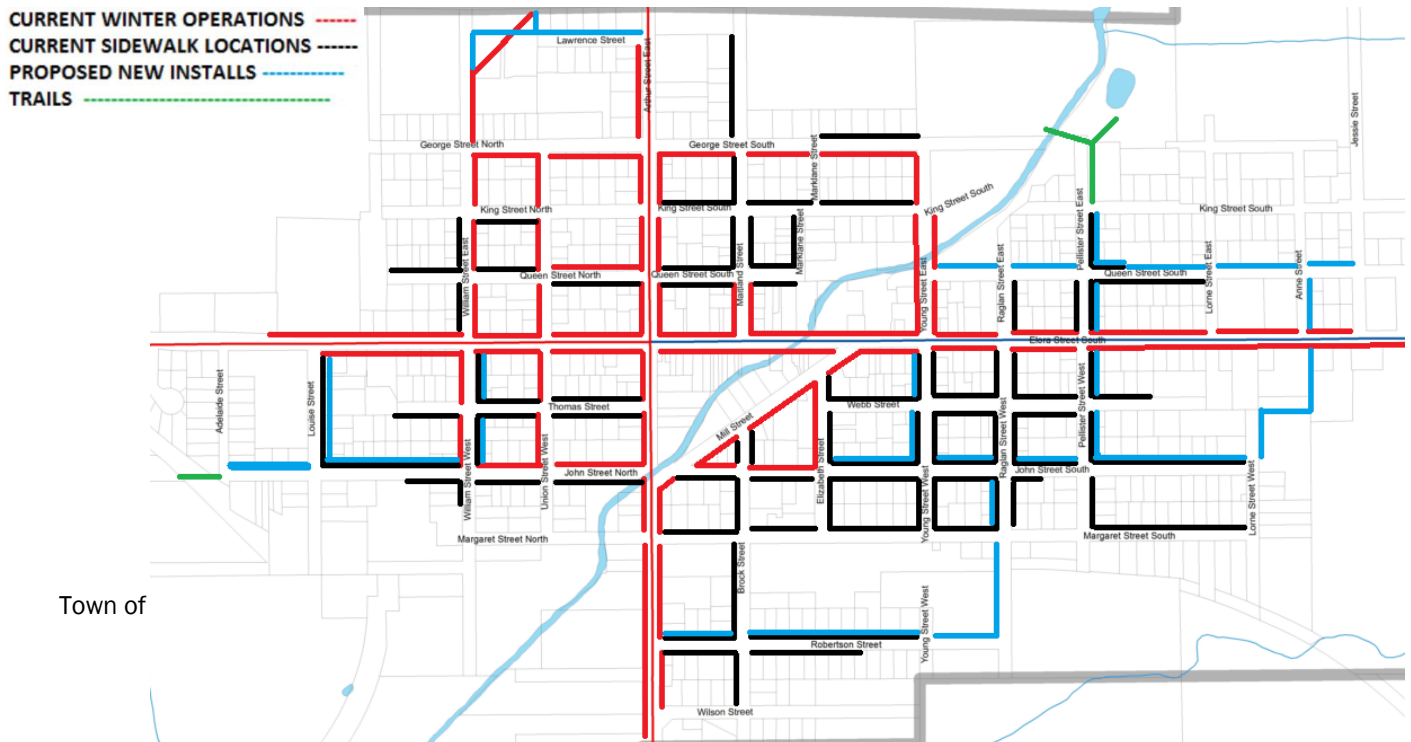
2.4 Sidewalk Inventory

The following is an inventory of existing and proposed sidewalks in Minto urban areas

Clifford



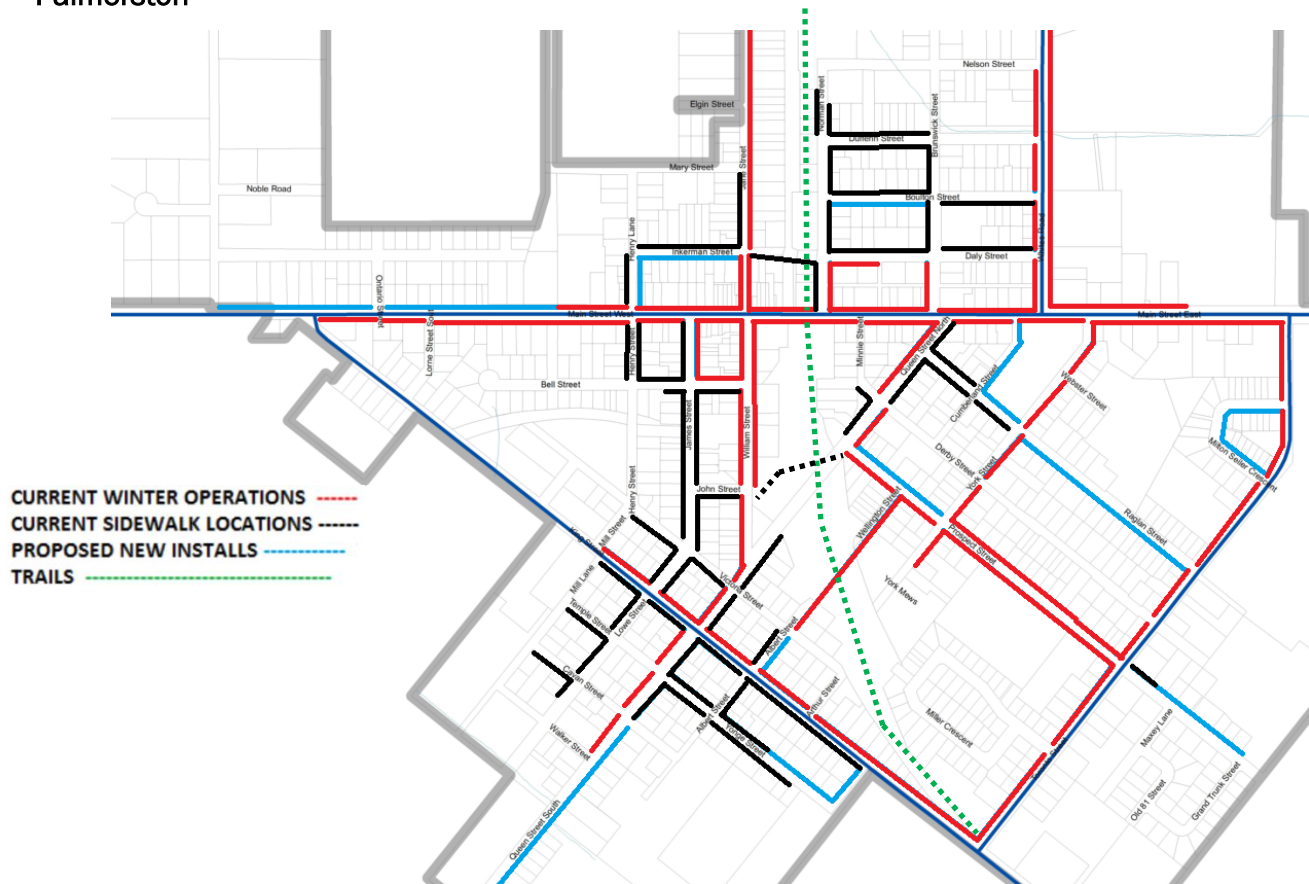
Harriston



Town of

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Palmerston



2.4 Trails

Trails are part of an integrated active transportation plan providing alternative pedestrian routes through communities. The Town has an active trail system within or adjacent to its urban areas, and sections of rural trail that link communities. Trails can act as an alternative to sidewalks depending on location and design, and can be used to join neighbourhoods through parkland or open space systems. Examples of trails include White's Junction Trail Palmerston, Greenway Trail Harriston and the Clifford Trail System.

3.0 Sidewalk Policies

The following are policies respecting Town of Minto sidewalks

- New sidewalks installed in urban areas shall be a minimum 1.5 metres wide and shall include all required accessibility elements as outlined in the County Design Manual and any other applicable standard (excerpt attached as Appendix "A" to this policy).
- Arterial roads such as Main Streets, Connecting Links and County roads through urban areas that carry high volumes of traffic will be designed or reconstructed with sidewalks on both sides.

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- c. Collector roads carrying traffic from neighbourhoods to arterial roads will be designed or constructed with a minimum one sidewalk on one side. Where sidewalks are on one side of a street the Town shall ensure a contiguous travel pattern through a neighbourhood to arterial roads or main community features such as schools, churches, retail areas
- d. Local roads carrying relatively low amounts of traffic to collector roads may have no sidewalk or may have a sidewalk on one or both sides if necessary to provide a continuous travel pattern through a neighbourhood or to link with community features such as schools, arenas, churches or neighbourhood commercial uses.
- e. Council may choose to enhance or reduce the standard of sidewalk installation for arterial, collector and local roads depending on physical conditions, changing land use, and community need based recommendations of Town staff or other qualified professionals.
- f. Existing substandard sidewalks according to County Design Standards located on arterial, collector or local roadways where the Town proposes to maintain a sidewalk shall be identified for replacement based on a condition and need assessment considering the following:
 - i) Substandard sidewalks on arterial roads shall be replaced before collector roads which shall be replaced before sidewalks on local roads.
 - ii) Sidewalks serving public institutions where there is heavy pedestrian use shall take priority over sidewalk sections that have less use and provide limited accessibility to community features.
 - ii) Substandard sidewalks in poor physical condition should be removed if they are unsafe for use or provide limited service benefit. The Town as its sole discretion may choose to remove an unsafe sidewalk and not replace it if the area would not normally be considered for sidewalk under this policy.
- g. In addition to moving forward removing and/or replacing substandard sidewalks where possible and practical, the Town shall work toward winter patrol and snow removal on all sidewalks on a phased and scheduled basis so that within 10 years all sidewalks in the Town comply with the standards in this policy and are maintained in the winter.
- h. The Town shall meet or exceed minimum maintenance standards for patrol, snow and ice accumulation, and encroachments on sidewalks, and in particular shall attempt to ensure snow accumulation does not exceed 2.5 cm depth with sand and salt within 12 hours of an event on direct routes to schools and on main roads.
- i. Town sidewalk patrol during the winter season shall occur at the same time road patrols occur and will include examination of no less than one sidewalk generally travelling north and south and one sidewalk generally travelling east west in all three urban areas.
- j. Trails may augment the sidewalk system in some cases and where this occurs the Town shall maintain the trail in accordance with this policy.

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- k. The maps attached as Appendix “B” to this policy as may be amended from time to time shall represent the desired sidewalk system in the Town.

4.0 Implementation

The Town shall fund the construction and maintenance of sidewalks in the following ways:

1. Developers of new subdivisions shall construct sidewalks at their sole cost as may be required within the subdivision agreement to comply with this policy. The Town shall assume maintenance responsibility of the sidewalk when it assumes the rest of the infrastructure within the project.
2. Within its capital budget the Town should include a minimum of \$60,000 annually for sidewalk repair and replacement beginning in the year of adoption of the policy increasing a minimum \$10,000 per year for the next 10 years.
3. Within its operating budget the Town should include a minimum of \$120,000 for winter and summer sidewalk maintenance.
4. Budget figures identified in this policy may be adjusted annually by Council upwards or downwards depending on infrastructure priorities.
5. This policy shall be considered annually during budget deliberations and shall be re-considered within every new term of Council.

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4.1 ACCESS AND CIRCULATION

DESIGN REQUIREMENTS (Continued)

Surfaces of curb ramps shall

- be slip-resistant;
- have a smooth transition from the curb ramp to adjacent surfaces; and
- incorporate a detectable warning surface comprised of directional tactile strips that
 - are configured as shown in 4.1.10.1;
 - are minimum 600 mm (23-5/8 in.) long, commencing 200 mm (7-7/8 in.) back from the edge of the vehicular roadway;
 - are parallel to the primary direction of pedestrian travel; and
 - guide pedestrians directly into a safe pedestrian crossing route across the roadway.

Where two sidewalks meet and the intersection is too narrow to incorporate tactile strips in both directions, tactile strips shall be located along the primary path of travel.

Curb ramps at pedestrian crosswalks shall be wholly contained within the area designated for pedestrian use.

Raised islands in crossings shall

- be cut through level with the street; or
- have curb ramps at both sides and a level area not less than 1370 long (54 in.) in the middle.

Islands level with the street shall have within the area designated for pedestrian use, detectable warning surfaces in compliance with Figure 4.1.10.1.

RELATED SECTIONS

- 4.1.1 Space and Reach Requirements
- 4.1.2 Ground and Floor Surfaces
- 4.4.8 Detectable Warning Surfaces
- 4.4.12 Glare and Light Sources
- 4.4.14 Materials and Finishes
- 4.4.15 Texture and Colour

4.1.10 CURB RAMPS

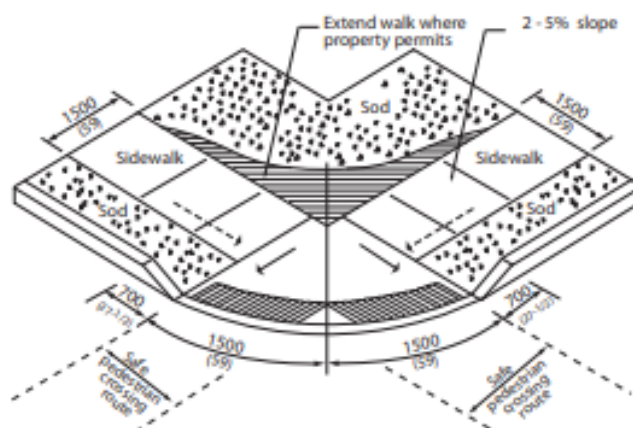


Figure 4.1.10.4
Curb Ramp at Sidewalks with Narrow Boulevards

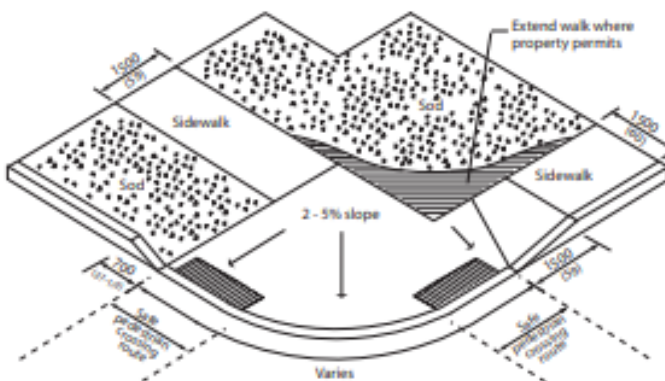


Figure 4.1.10.5
Curb Ramp where Sidewalk at Curb
meets Sidewalk with Wide Boulevard

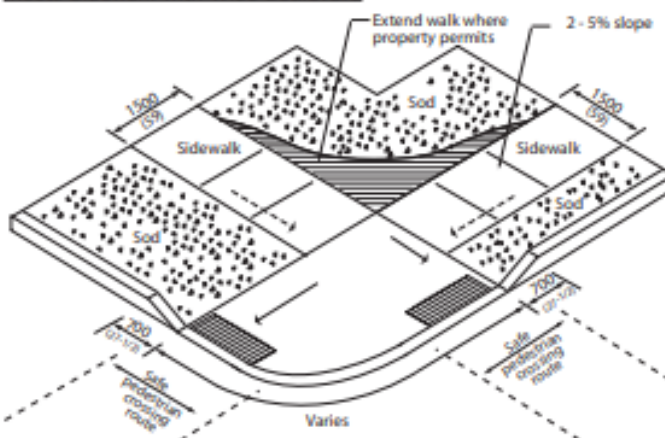
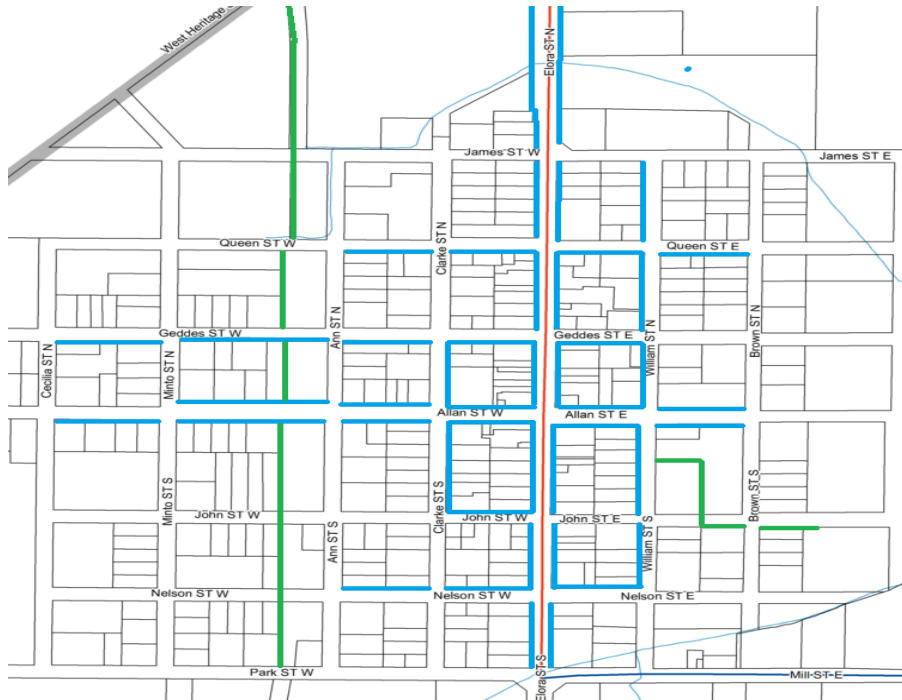


Figure 4.1.10.6
Curb Ramp where Sidewalk with Narrow Boulevard
meets Sidewalk with Wide Boulevard

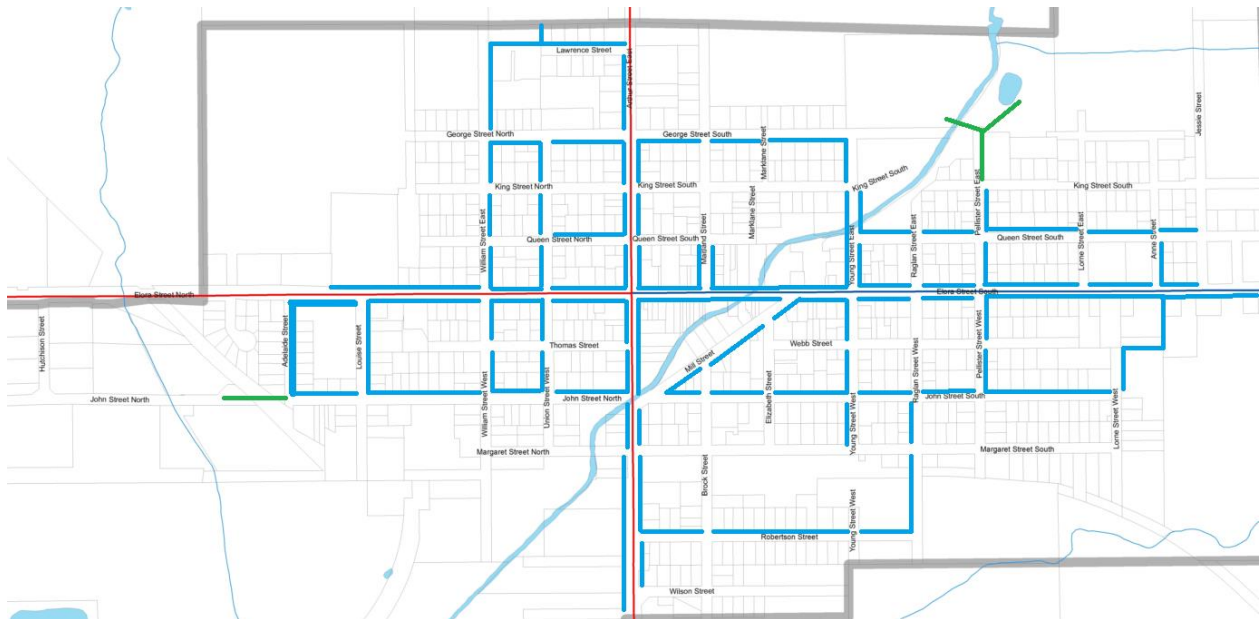
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Appendix “B”

Clifford Future Sidewalk System

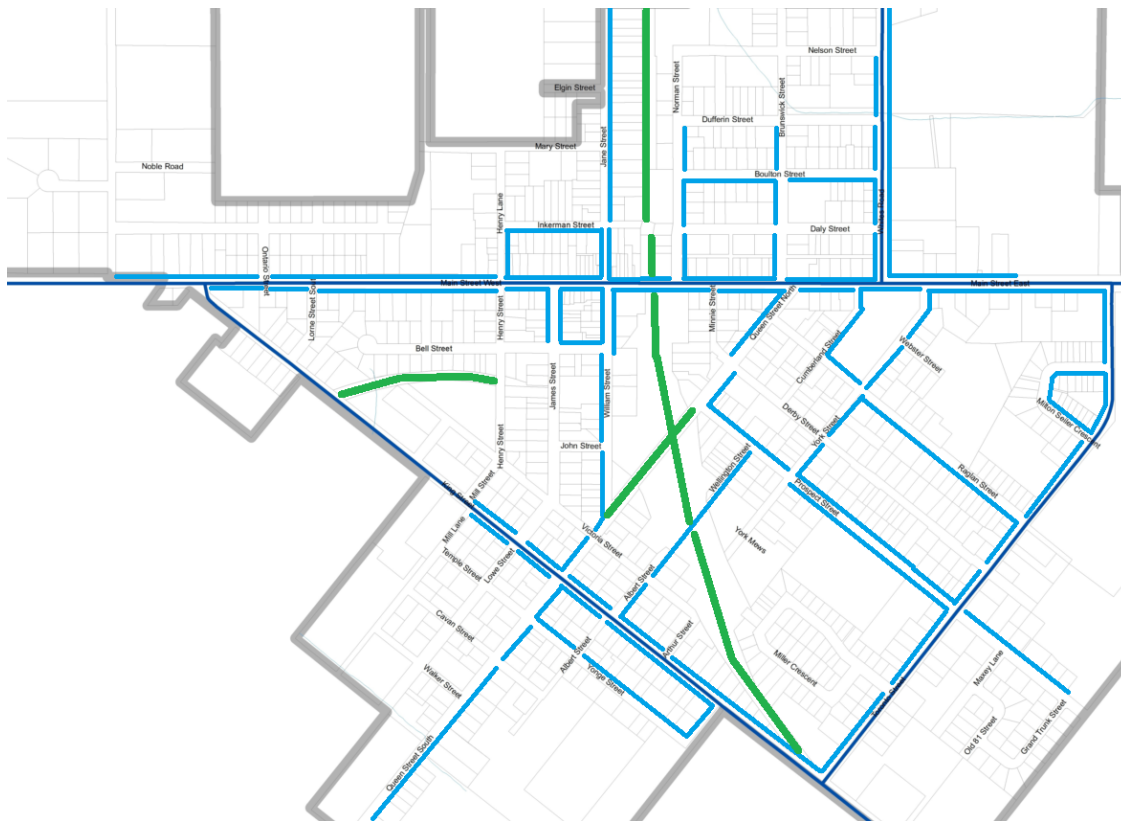


Harriston Future Sidewalk System



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Palmerston Future Sidewalk System



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